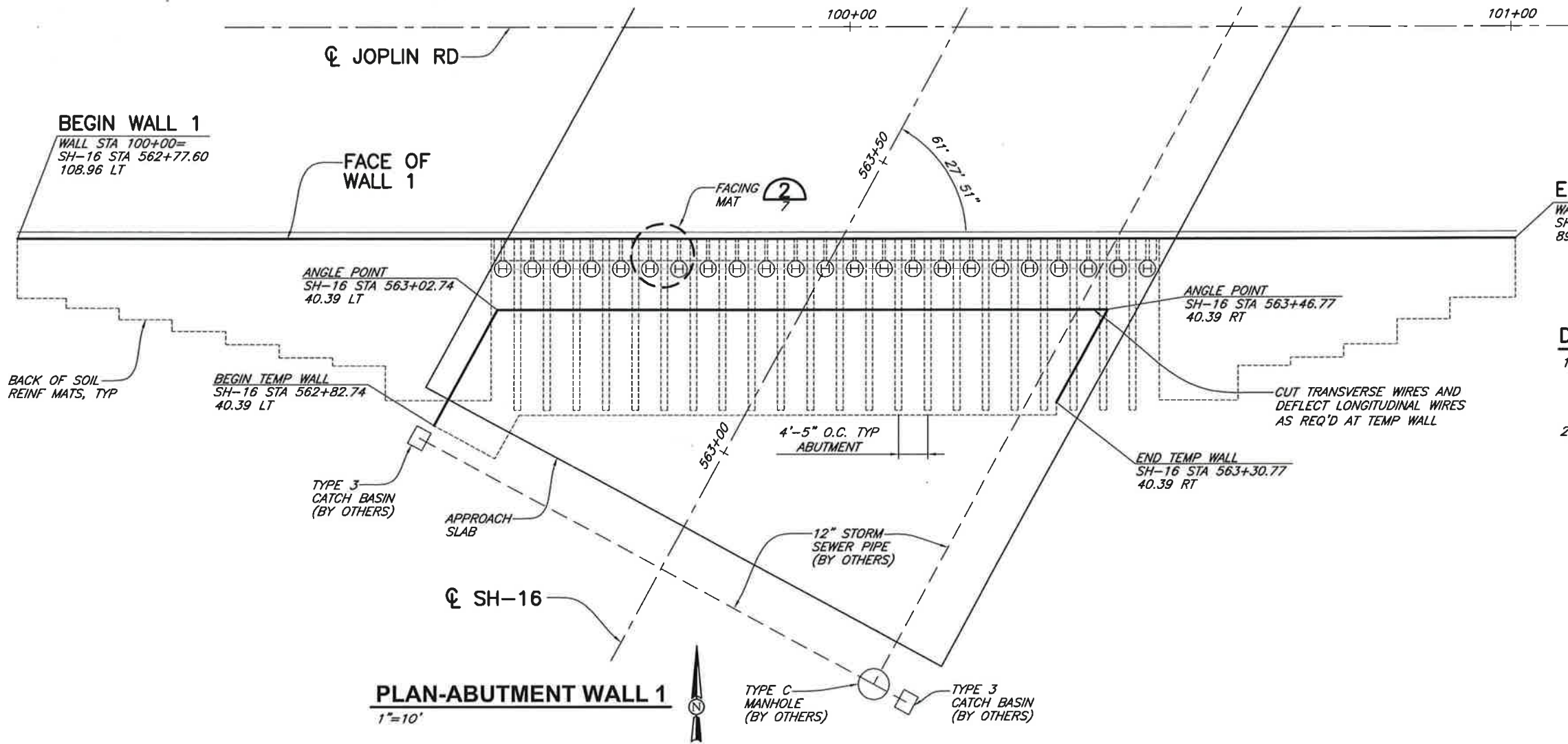


CAD: \\ZING\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R3.DWG  
(HRW 120613 AE)



**PLAN-ABUTMENT WALL 1**  
1"=10'

**END WALL 1**  
WALL STA 102+26=  
SH-16 STA 563+85.76  
89.48 RT

**DESIGN NOTES**

- DESIGN IS BASED ON THE ASSUMPTION THAT BACKFILL WITHIN THE REINFORCED SOIL MASS, METHODS OF CONSTRUCTION AND QUALITY OF MATERIALS CONFORM TO THE REQUIREMENTS OF HILFIKER RETAINING WALLS.
- ASSUMED SOIL CHARACTERISTICS:  
 WALL BACKFILL (CONTRACTOR TO VERIFY BY TEST):  
 UNIT WEIGHT: 125 PCF  
 INTERNAL FRICTION ANGLE: 34°  
 COHESION: 0 PSF  
 RANDOM BACKFILL:  
 UNIT WEIGHT: 125 PCF  
 INTERNAL FRICTION ANGLE: 34°  
 COHESION: 0 PSF  
 COEFFICIENT OF ACTIVE SOIL PRESSURE: 0.283  
 FOUNDATION SOILS:  
 FRICTION ANGLE FOR SLIDING: 32°  
 COHESION: 0 PSF  
 MAXIMUM APPLIED BEARING PRESSURE:  
 STRENGTH LIMIT: 8.9 KSF  
 EXTREME LIMIT: 12.3 KSF  
 SERVICE LIMIT: 6.24 KSF  
 IF ACTUAL CHARACTERISTICS, GRADES OR DIMENSIONS OF SOIL MATERIALS DIFFER FROM THOSE LISTED ABOVE OR SHOWN ON THE PLANS HILFIKER RETAINING WALLS SHALL BE NOTIFIED TO EVALUATE THE NEED TO REDESIGN.
- THE DESIGN REQUIRES A NON-SATURATED BACKFILL. SURFACE AND SUB-SURFACE DRAINAGE CONTROL MAY BE REQUIRED TO PREVENT SATURATION OF THE BACKFILL OR RELIEVE HYDROSTATIC PRESSURES.  
 DRAINAGE CONTROL SHALL BE AS SPECIFIED IN THE PROJECT PLANS AND SPECIFICATIONS OR AS DIRECTED BY THE OWNER'S ENGINEER. PAYMENT FOR DRAINAGE SHALL BE AS SPECIFIED IN THE PROJECT SPECIFICATIONS.
- SOIL REINFORCEMENTS SHALL BE FABRICATED OF WELDED WIRE FABRIC CONFORMING TO AASHTO M-32 AND M-55 AND SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M-111 FOLLOWING FABRICATION.
- REFERENCE DOCUMENTS:  
 ELECTRONIC FILES IN .PDF FORMAT, PREPARED BY IDAHO TRANSPORTATION DEPARTMENT, PARAMETRIX. DRAWINGS DATED APRIL 2012.

**TEMPORARY WALL QUANTITIES**

BASE DEPTH	CAP MAT W4.5xW3.5	PRONGLESS MAT W4.5xW3.5	STANDARD MAT W4.5xW4 (8x21)
16'-0"	23	23	-
15'-9"	-	-	109
10'-6"	-	-	16
10'-0"	9	9	-
WALL FACE SUPPLIED		1,296 SQ FT	
MSE BACKFILL (BY OTHERS)		*694 CU YD	
BACKING MATS 2' HIGH		77 EA	
BACKING MATS 3' HIGH		5 EA	
FILTER FABRIC 2'-6" HIGH		652 LIN FT	
HOG RINGS		1,900 EA	
PLIERS		2 EA	

\* NEAT, TOP MAT TO BOTTOM MAT

**ABUTMENT 1 QUANTITIES**

BASE DEPTH	CAP MAT W14xWB (4x8)	PRONGLESS MAT W14xWB (4x8)	STANDARD MAT W14xWB (4x12)	STANDARD MAT W10xW7 (4x12)	CAP MAT W4.5xW3.5 (8x12)	PRONGLESS MAT W4.5xW3.5 (8x12)	STANDARD MAT W7xW4 (8x21)	STANDARD MAT W9.5xW4 (8x21)
26'-0"	22	22	163	88	-	-	-	-
24'-6"	-	-	-	-	-	-	72	12
24'-0"	-	-	-	-	7	7	-	-
19'-3"	-	-	-	-	-	-	45	-
19'-0"	-	-	-	-	4	4	-	-
18'-0"	-	-	-	-	4	4	-	-
17'-6"	-	-	-	-	-	-	38	-
16'-0"	-	-	-	-	4	4	-	-
15'-9"	-	-	-	-	-	-	31	-
14'-0"	-	-	-	-	2	2	13	-
12'-3"	-	-	-	-	-	-	22	-
12'-0"	-	-	-	-	4	4	-	-
10'-6"	-	-	-	-	-	-	8	-
10'-0"	-	-	-	-	2	2	-	-
9'-0"	-	-	-	-	5	5	-	-
8'-9"	-	-	-	-	-	-	17	-
4'-0"	-	-	-	-	23	23	-	-
3'-6"	-	-	-	-	-	-	263	-
WALL FACE SUPPLIED	6,673 SQ FT		CLOSURE MATS 8"x12"x4'-0"	57 EA				
MSE BACKFILL (BY OTHERS)	*2,692 CU YD		FILTER FABRIC 7'-6" HIGH	1,713 LIN FT				
BACKING MATS 2' HIGH	363 EA		FILTER FABRIC 2'-6" HIGH	2,445 LIN FT				
BACKING MATS 3' HIGH	6 EA		HOG RINGS	8,600 EA				
			PLIERS	4 EA				

\* NEAT, TOP MAT TO BOTTOM MAT



EXPIRES: 2/28/2013

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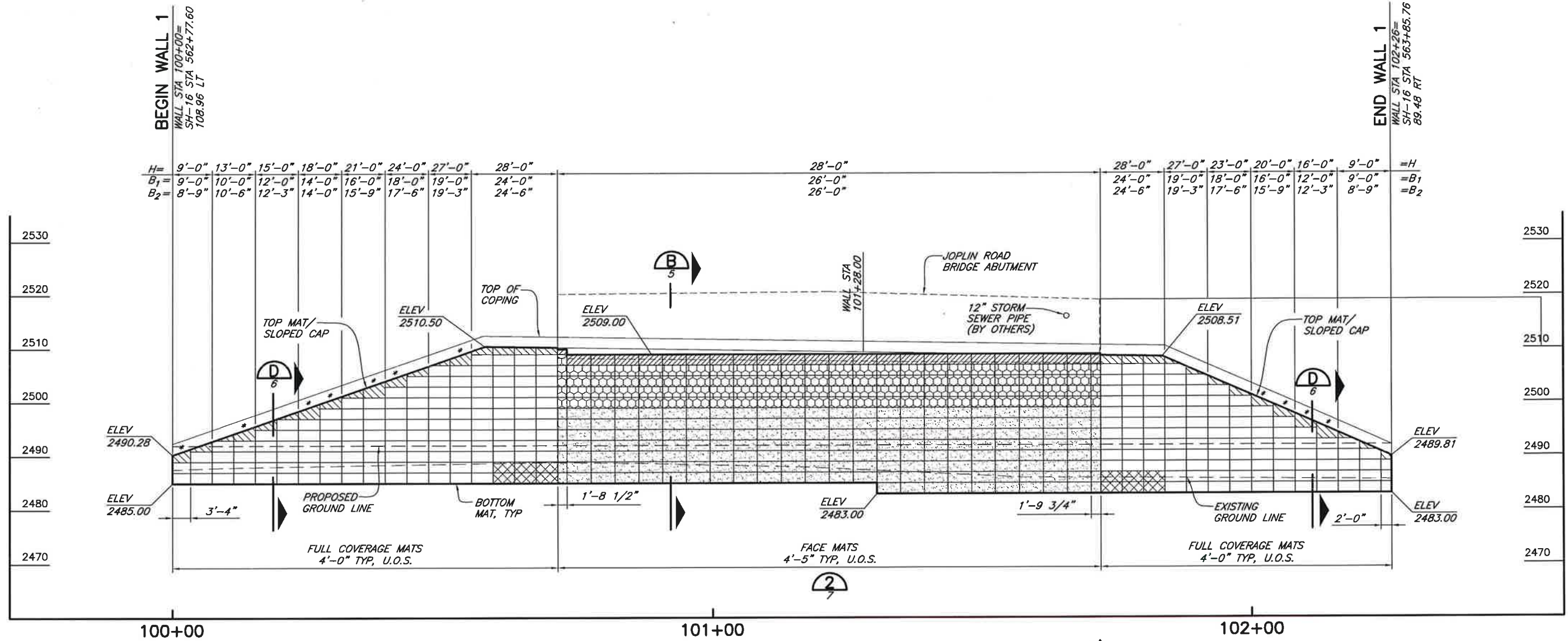
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12/13/12	1	Final revisions
11/14/12	2	Review comments
10/8/12	3	Review comments

DESIGN	CHK	APVD
K/JN	K/JN	K/JN

SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD  
IDAHO DEPARTMENT OF TRANSPORTATION  
MERRIDIAN, IDAHO

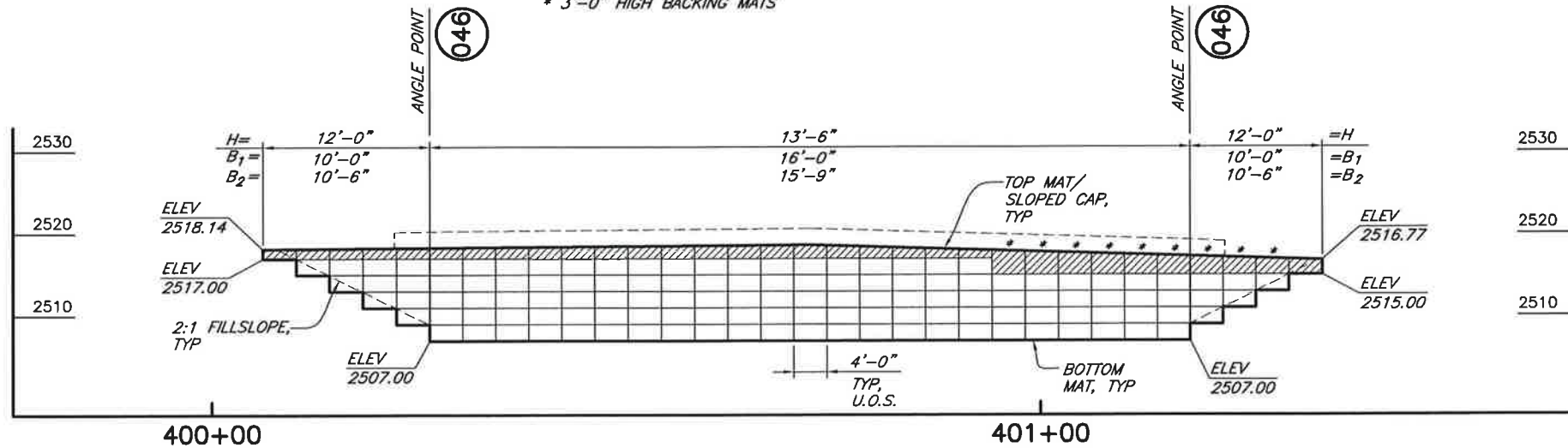
**ABUTMENT WALL 1  
PLAN & NOTES**

CAD: \\ZING\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R3.DWG  
(HRW 120613 AE)



**WALL AT ABUTMENT 1 DEVELOPED ELEVATION (BACKFACE)**

1"=10' HORIZ AND VERT  
U.O.S.= UNLESS OTHERWISE STATED  
\* 3'-0" HIGH BACKING MATS



**TEMPORARY WALL DEVELOPED ELEVATION (BACKFACE)**

1"=10' HORIZ AND VERT  
U.O.S.= UNLESS OTHERWISE STATED  
\* 3'-0" HIGH BACKING MATS

**WWR MAT LEGEND**

- 4x8, W14xW8 WWR SOIL REINFORCING MATS
- 4x12, W10xW7 WWR SOIL REINFORCING MATS
- 4x12, W14xW8 WWR SOIL REINFORCING MATS
- 8x12, W4.5xW3.5 WWR SOIL REINFORCING MATS
- 8x21, W7xW4 WWR SOIL REINFORCING MATS
- 8x21, W9.5xW4 WWR SOIL REINFORCING MATS OR 8x21, W7xW4 STD MATS WITH 12X24, W8xW4 INTERMEDIATE FLAT MATS

**WWR MAT LEGEND**

- 8x12, W4.5xW3.5 WWR SOIL REINFORCING MATS
- 8x21, W4.5xW4 WWR SOIL REINFORCING MATS



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NO.	DATE	BY	REVISION
12/13/12		K/JN	Final revisions
11/14/12		K/JN	Review comments
10/18/12		K/JN	Review comments

DESIGN: K/JN  
DRAWING: MMD  
CHECK: K/JN  
APPROVED: APVD

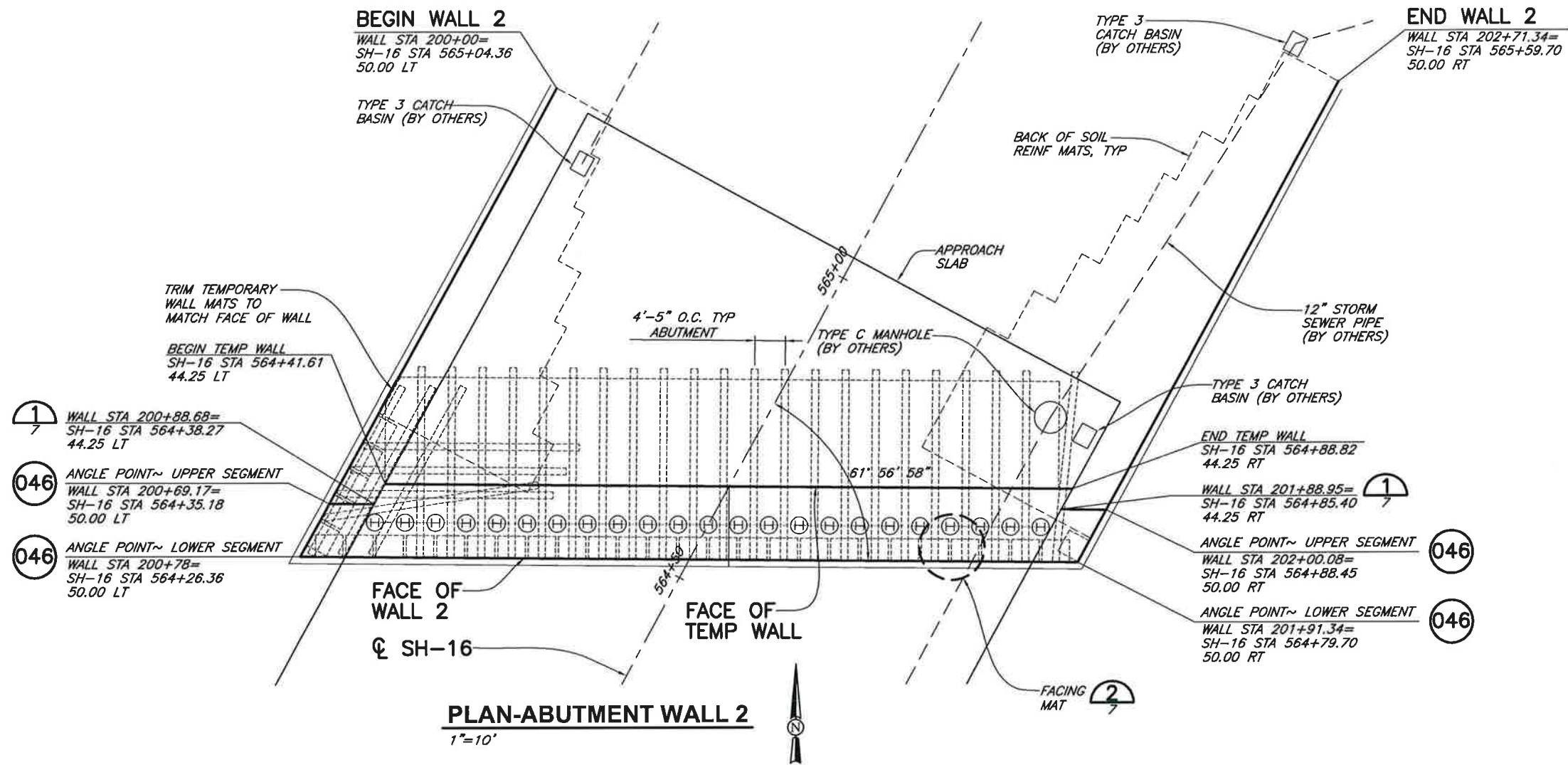
SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD  
IDAHO DEPARTMENT OF TRANSPORTATION  
MERIDIAN, IDAHO

**ABUTMENT WALL 1  
ELEVATION**

SHEET 2 OF 10  
DATE 8/30/12  
PROJ. NO. 012167

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(HRW 120613 AE)



**ABUTMENT 2 QUANTITIES** ③

BASE DEPTH	CAP MAT W14xW8 (4x8)	PRONGLESS MAT W14xW8 (4x8)	STANDARD MAT W14xW8 (4x12)	STANDARD MAT W10xW7 (4x12)	CAP MAT W4.5xW3.5 (8x12)	PRONGLESS MAT W4.5xW3.5 (8x12)	STANDARD MAT W7xW4 (8x21)	STANDARD MAT W9.5xW4 (8x21)
28'-0"	29	29	261	135	7	6	58	30
24'-6"	-	-	-	-	-	-	44	16
24'-0"	-	-	-	-	4	4	-	-
21'-0"	-	-	-	-	4	4	44	8
18'-0"	-	-	-	-	4	4	-	-
17'-6"	-	-	-	-	-	-	42	-
16'-0"	-	-	-	-	4	4	-	-
15'-9"	-	-	-	-	-	-	34	-
14'-0"	-	-	-	-	2	2	12	-
12'-3"	-	-	-	-	-	-	14	-
12'-0"	-	-	-	-	2	2	-	-
10'-6"	-	-	-	-	-	-	18	-
10'-0"	-	-	-	-	4	4	-	-
9'-0"	-	-	-	-	4	4	-	-
8'-9"	-	-	-	-	-	-	9	-
4'-0"	-	-	-	-	34	34	-	-
3'-6"	-	-	-	-	-	-	441	-
WALL FACE SUPPLIED	9,614 SQ FT		CLOSURE MATS 8"x12"x4'-0"	68 EA				
MSE BACKFILL (BY OTHERS)	*4,109 CU YD		FILTER FABRIC 7'-6" HIGH	2,645 LIN FT				
BACKING MATS 2' HIGH	512 EA		FILTER FABRIC 2'-6" HIGH	3,590 LIN FT				
BACKING MATS 3' HIGH	9 EA		HOG RINGS	12,700 EA				
			PLIERS	5 EA				

\* NEAT, TOP MAT TO BOTTOM MAT

**TEMPORARY WALL QUANTITIES**

BASE DEPTH	CAP MAT W4.5xW3.5	PRONGLESS MAT W4.5xW3.5	STANDARD MAT W4.5xW4 (8x12)	STANDARD MAT W4.5xW4 (8x21)
16'-0"	26	26	-	-
15'-9"	-	-	-	118
4'-4 1/2"	3	3	13	-
WALL FACE SUPPLIED	1,184 SQ FT			
MSE BACKFILL (BY OTHERS)	*674 CU YD			
BACKING MATS 2' HIGH	71 EA			
BACKING MATS 3' HIGH	4 EA			
FILTER FABRIC 2'-6" HIGH	600 LIN FT			
HOG RINGS	1,800 EA			
PLIERS	2 EA			

\* NEAT, TOP MAT TO BOTTOM MAT

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**SAW**

REGISTERED PROFESSIONAL ENGINEER  
No. 10301  
JEFF NELSON

EXPIRES: 2/28/2013

REVISION	DATE	BY	REVISION
Final revisions	12/13/12	K/JN	
Review comments	11/14/12	K/JN	
Review comments	10/18/12	K/JN	

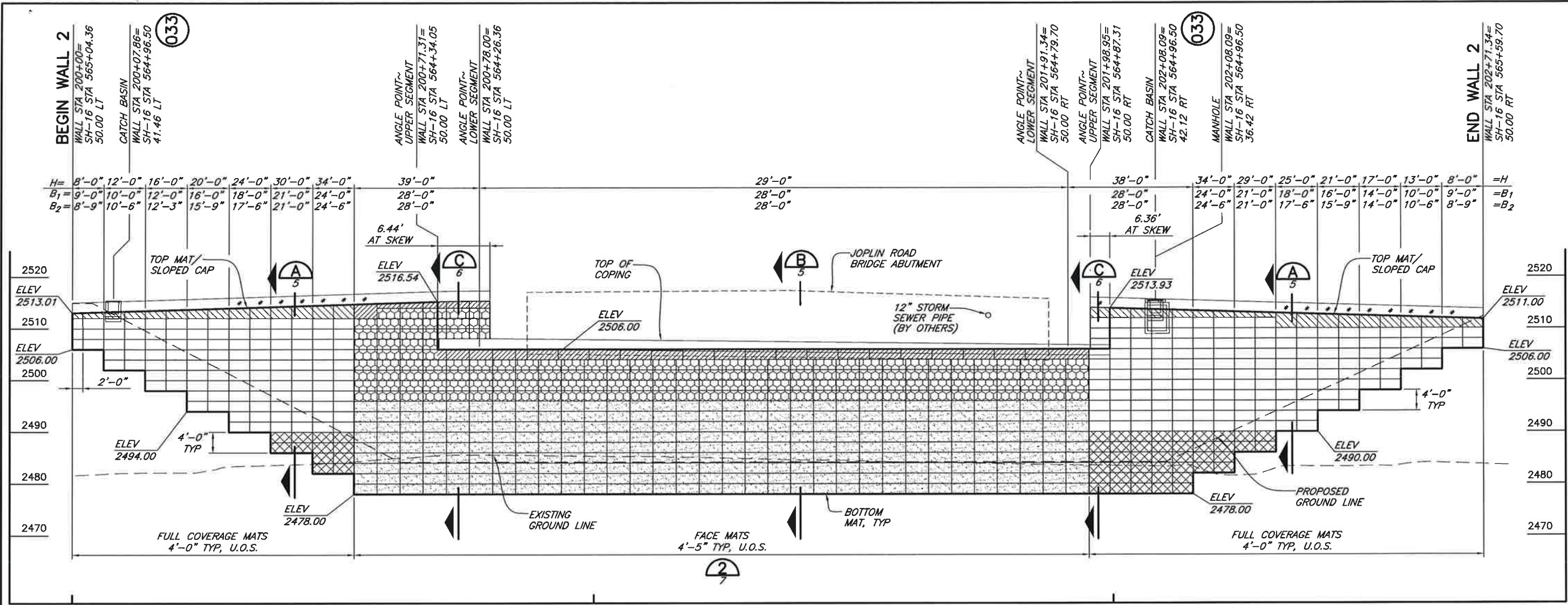
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DR: MMD  
CHK: K/JN  
APVD:

SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD  
IDAHO DEPARTMENT OF TRANSPORTATION  
MERIDIAN, IDAHO

**ABUTMENT WALL 2 PLAN**

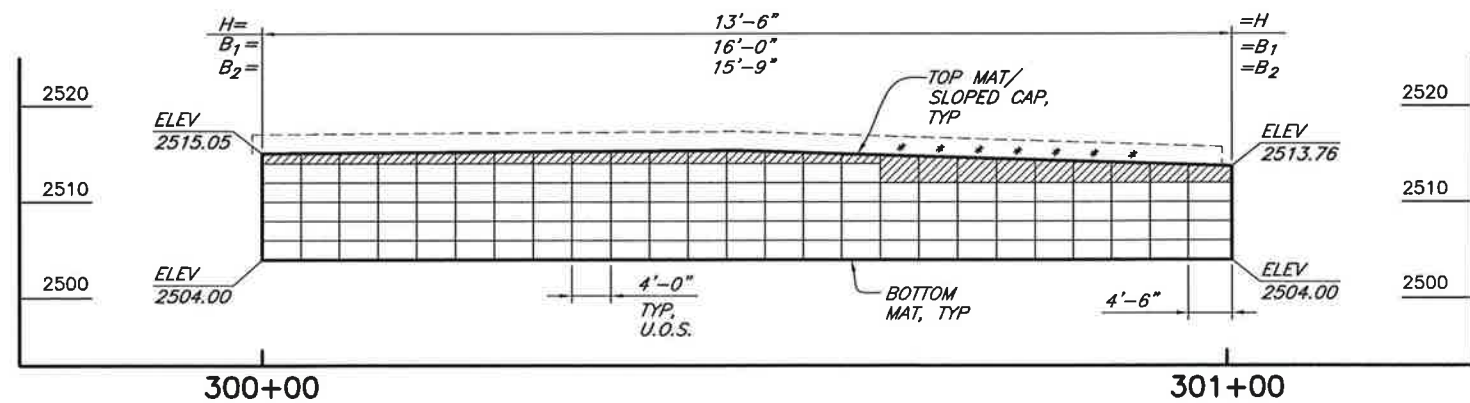
SHEET 3 OF 10  
DATE 8/30/12  
PROJ. NO. 012167

CAD: \\ZING\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R3.DWG  
(HRW 120613 AE)



**WALL AT ABUTMENT 2 DEVELOPED ELEVATION**

1"=10' HORIZ AND VERT  
U.O.S.= UNLESS OTHERWISE STATED  
\* 3'-0" HIGH BACKING MATS



**TEMPORARY WALL DEVELOPED ELEVATION**

1"=10' HORIZ AND VERT  
U.O.S.= UNLESS OTHERWISE STATED  
\* 3'-0" HIGH BACKING MATS

**WWR MAT LEGEND**

- 4x8, W14xW8 WWR SOIL REINFORCING MATS
- 4x12, W10xW7 WWR SOIL REINFORCING MATS
- 4x12, W14xW8 WWR SOIL REINFORCING MATS
- 8x12, W4.5xW3.5 WWR SOIL REINFORCING MATS
- 8x21, W7xW4 WWR SOIL REINFORCING MATS
- 8x21, W9.5xW4 WWR SOIL REINFORCING MATS OR 8x21, W7xW4 STD MATS WITH 12X24, W8xW4 INTERMEDIATE FLAT MATS

**WWR MAT LEGEND**

- 8x12, W4.5xW3.5 WWR SOIL REINFORCING MATS
- 8x21, W4.5xW4 WWR SOIL REINFORCING MATS

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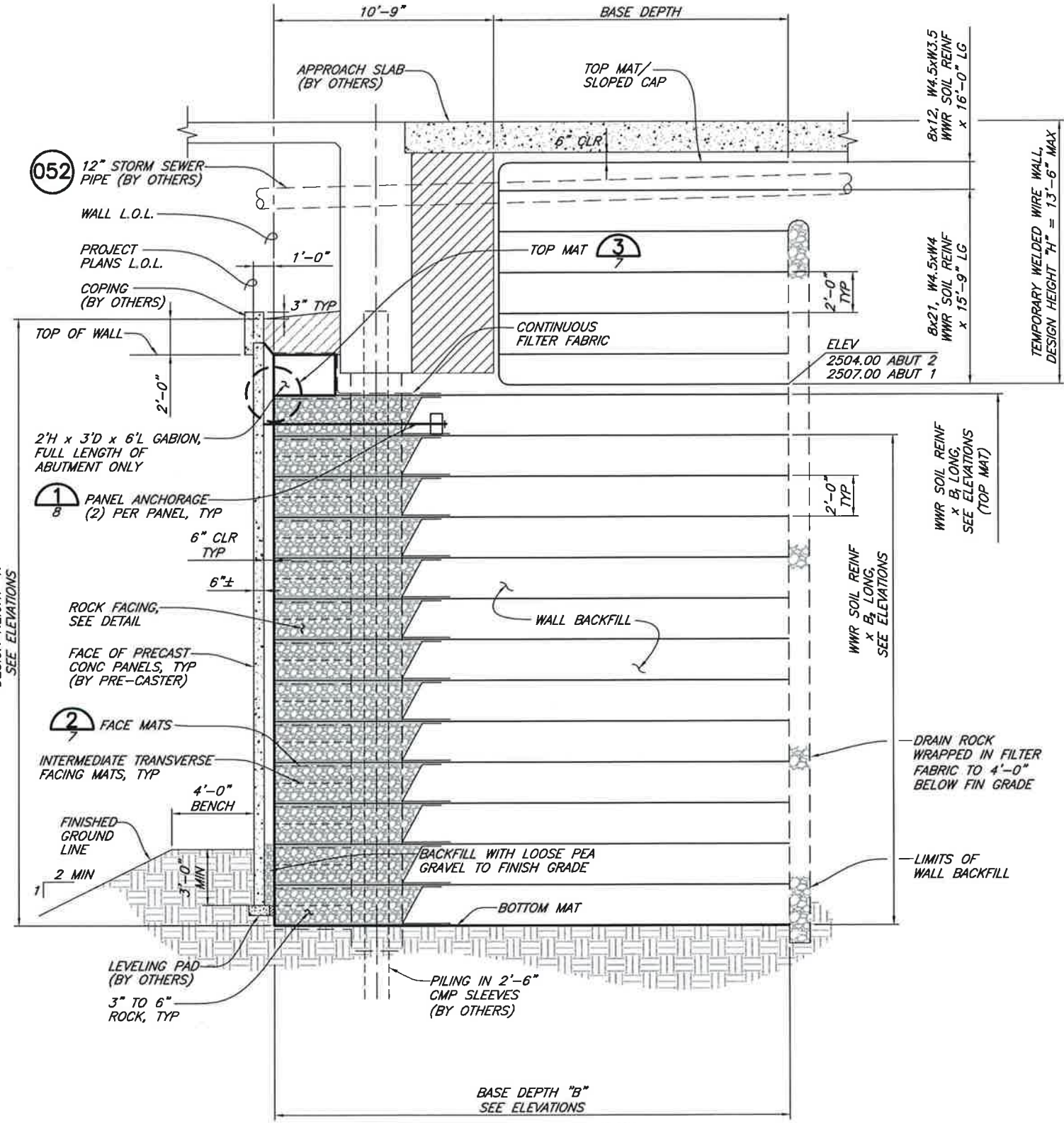
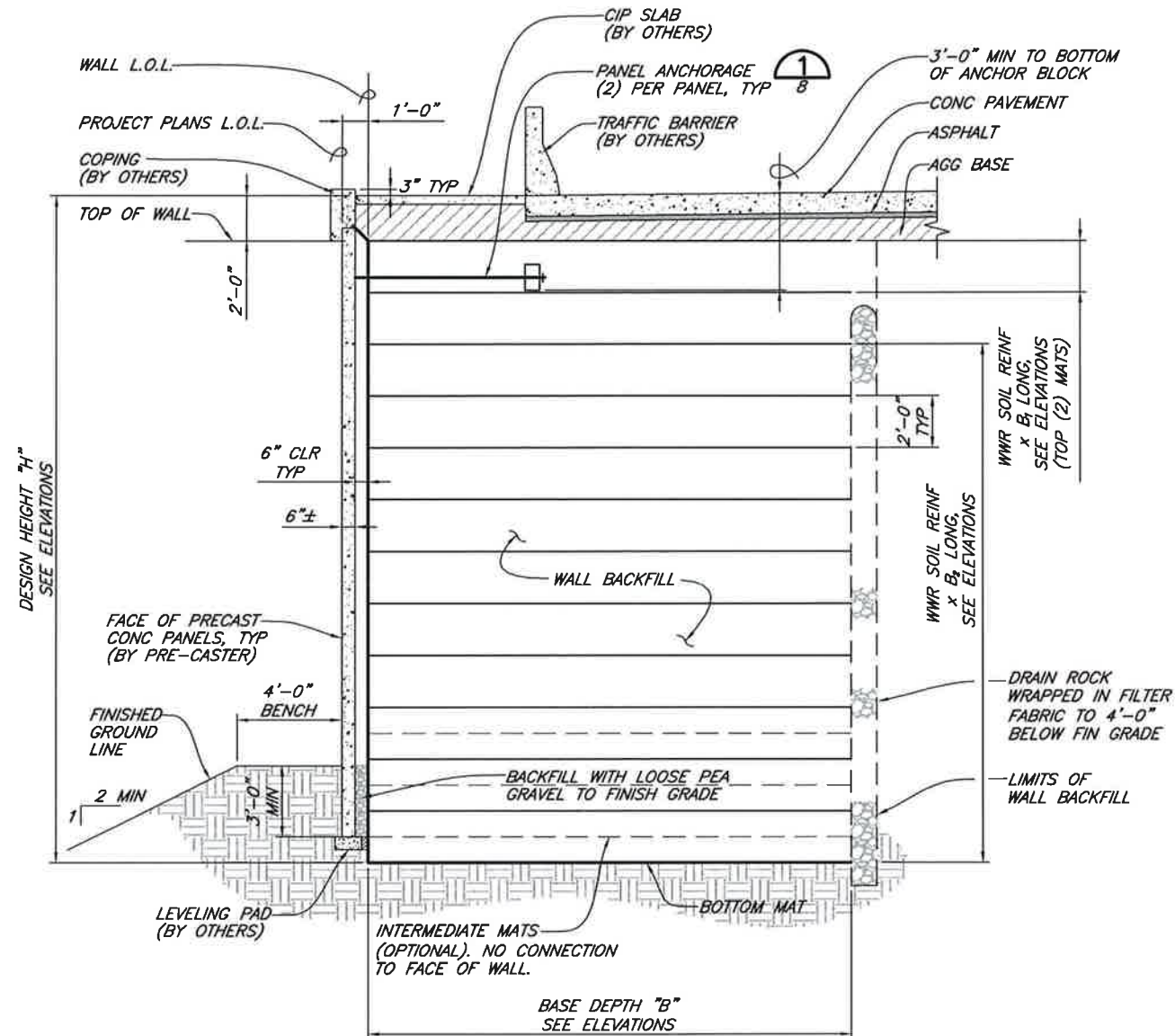
**SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD**  
IDAHO DEPARTMENT OF TRANSPORTATION  
MERIDIAN, IDAHO

**ABUTMENT WALL 2 ELEVATION**

DESIGN	K/JN	DR	MMD	CHK	K/JN	APVD	
DATE	12/13/12	NO.					
REVISION		Final revisions	Review comments				

SHEET 4 OF 10  
DATE 8/30/12  
PROJ. NO. 012167

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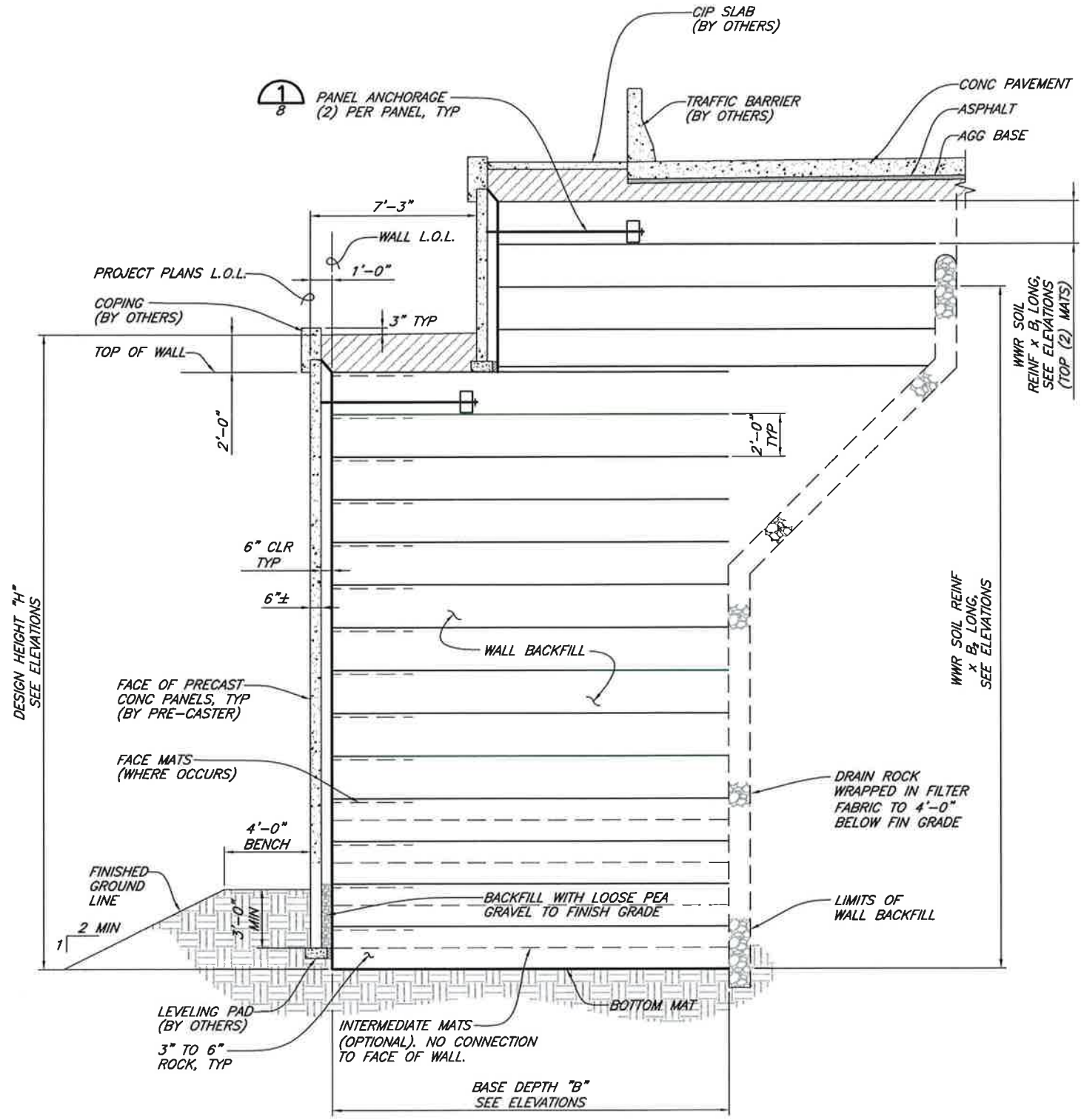
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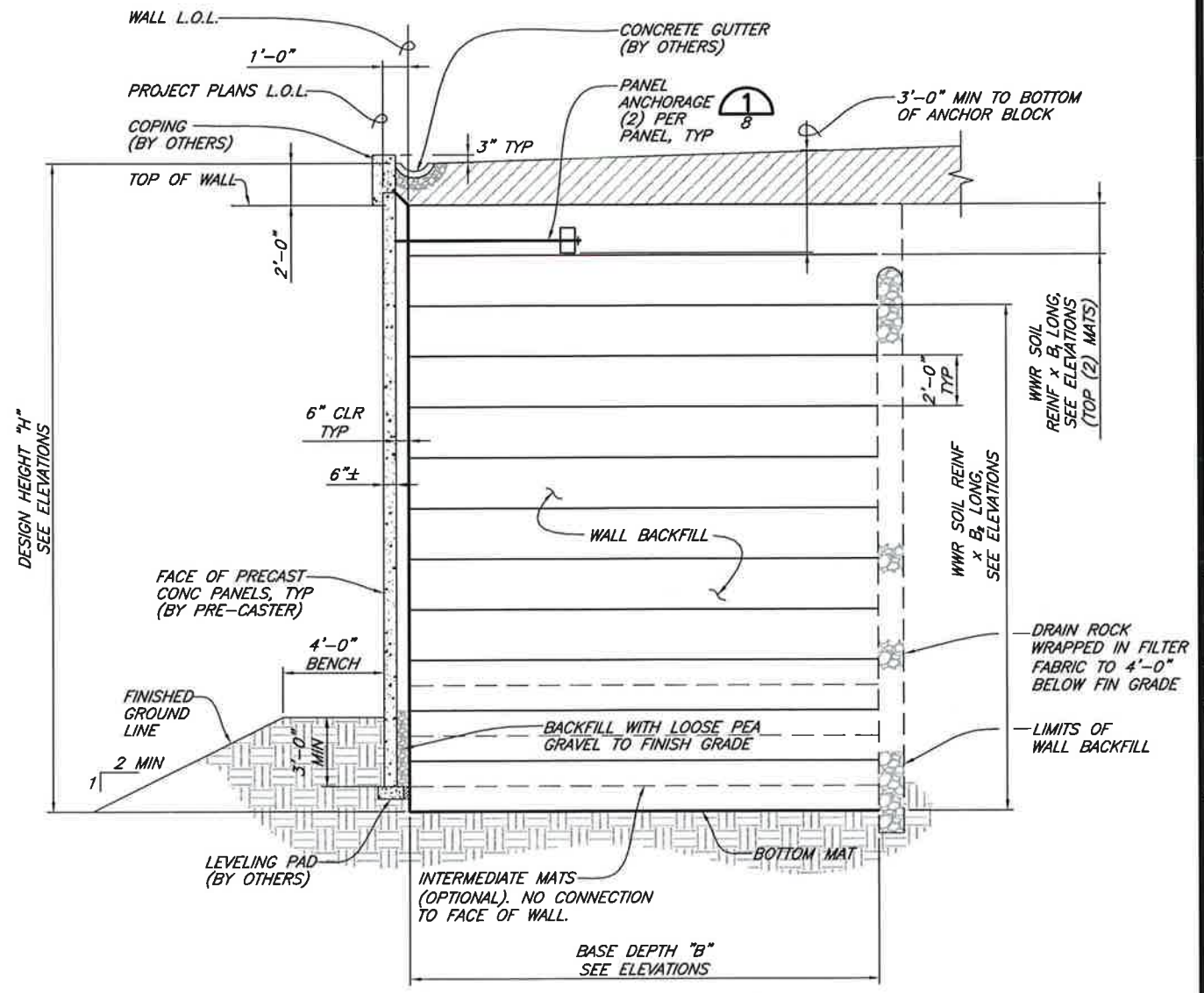


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<b>SECTIONS</b>			
DESIGN	DR	CHK	APVD
K/JN	MMD	K/JN	
Final revisions	Review comments	Review comments	REVISION
12/3/12	11/14/12	10/8/12	DATE
NO.	NO.	NO.	NO.
SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD IDAHO DEPARTMENT OF TRANSPORTATION MERIDIAN, IDAHO			
SHEET 5 OF 10			
DATE 8/30/12			
PROJ. NO. 012167			

CAD: \\ZING\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R3.DWG  
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SECTION C  
NO SCALE



SECTION D  
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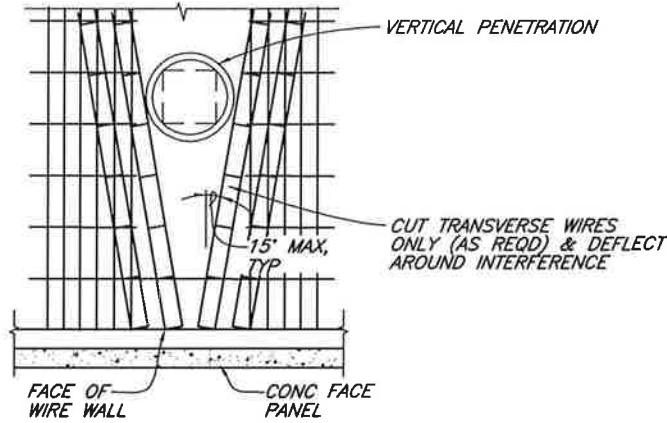
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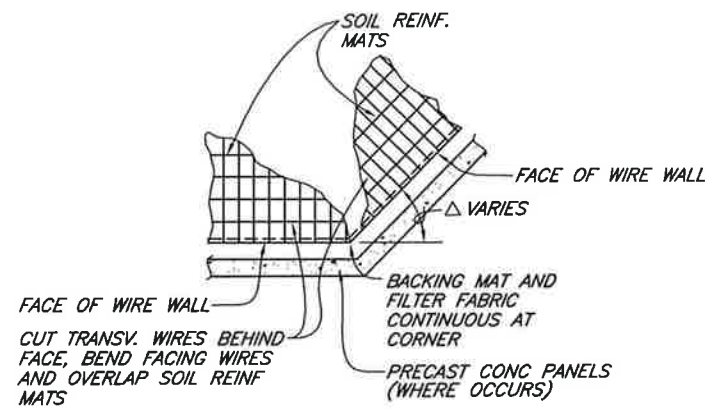
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<b>SH</b>	
DESIGN	NO.
DR	NO.
CHK	NO.
APVD	NO.
DATE	REVISION
12/3/12	Final revisions
11/14/12	Review comments
10/8/12	Review comments
SECTIONS	
SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD IDAHO DEPARTMENT OF TRANSPORTATION MERIDIAN, IDAHO	
SHEET	6 OF 10
DATE	8/30/12
PROJ. NO.	012167



**SOIL REINF. PENETRATION**

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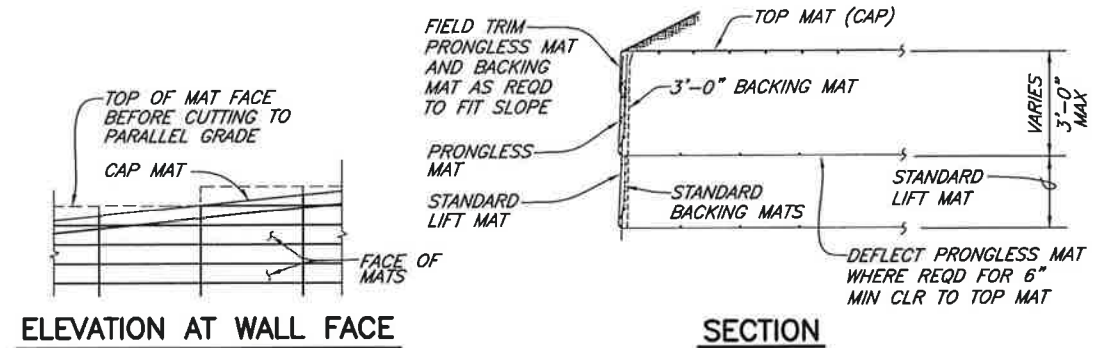
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**CONVEX ANGLE POINT**

NO SCALE

046



**ELEVATION AT WALL FACE**

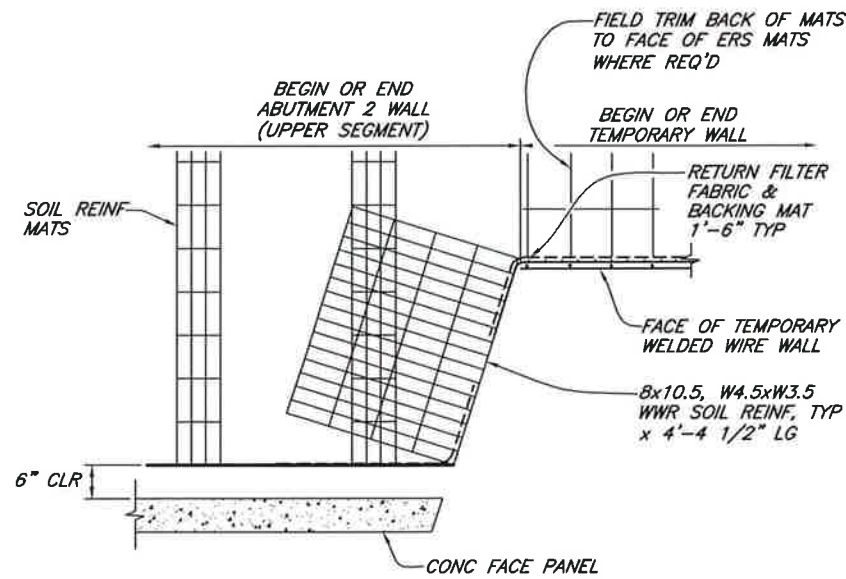
**SECTION**

**INSTALLATION SEQUENCE**

1. PLACE THE PRONGLESS MATS AND BACKING MATS FOR THE TOP LIFTS.
2. CUT THE PRONGLESS AND BACKING MAT FACES OFF PARALLEL TO THE SLOPE OF THE FINAL GRADE.
3. PLACE AND COMPACT THE BACKFILL IN THE TOP LIFTS TO PARALLEL THE FINAL GRADE SLOPE.
4. PLACE THE CAPS ON THE BACKFILL AND CLIP THEM TO THE PRONGLESS MAT FACES WITH HOG RINGS.
5. PLACE AND COMPACT THE FINAL COVER OVER THE CAP MATS.

**SLOPED CAP DETAILS**

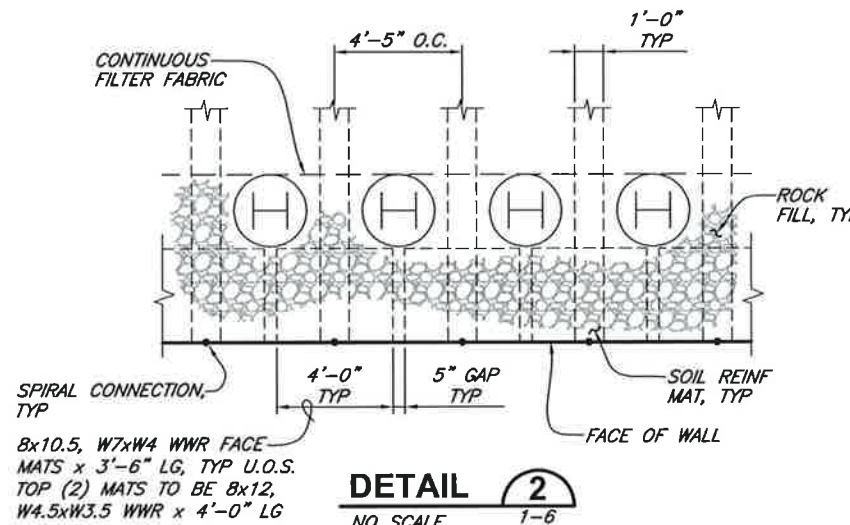
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**DETAIL 1**

NO SCALE

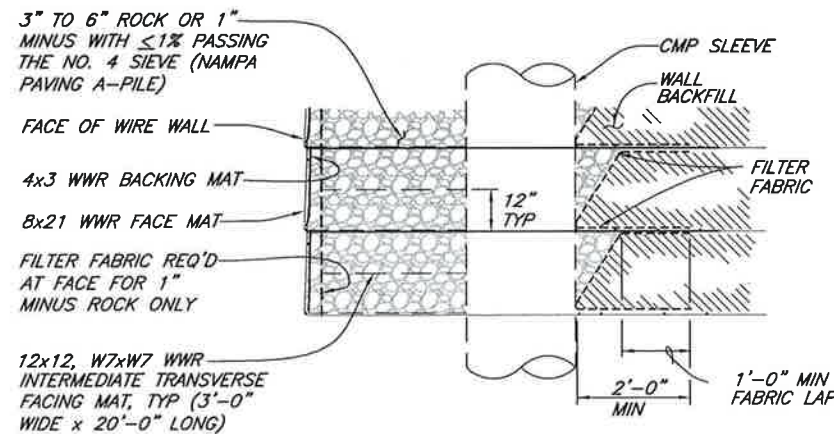
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**DETAIL 2**

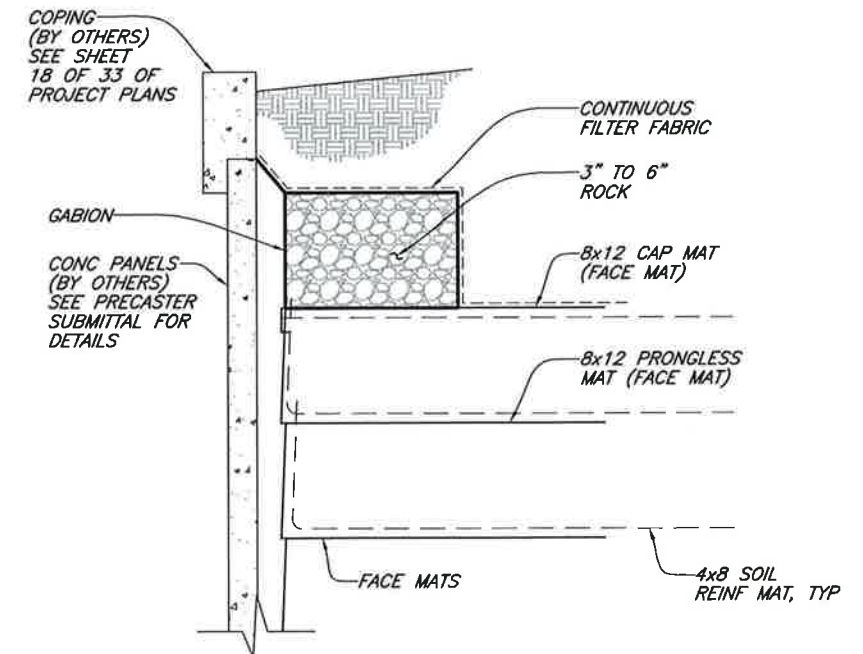
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1-6



**ROCK FACING**

NO SCALE



**DETAIL 3**

NO SCALE

5

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WEB SITE www.hilfiKER.com E-MAIL info@hilfiKER.com

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**SM**

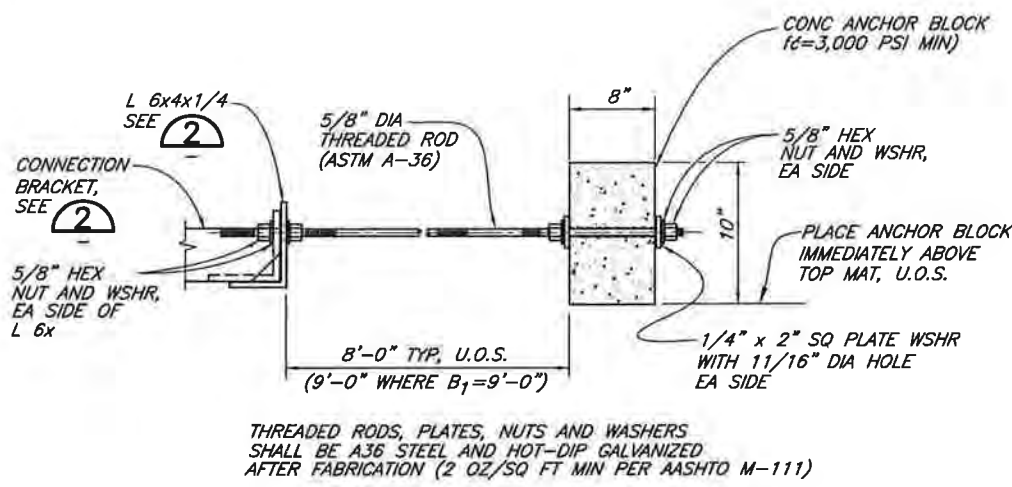
NO.	DATE	REVISION
1	12/13/12	Final revisions
2	11/14/12	Review comments
3	10/18/12	Review comments

SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD  
IDAHO DEPARTMENT OF TRANSPORTATION  
MERIDIAN, IDAHO

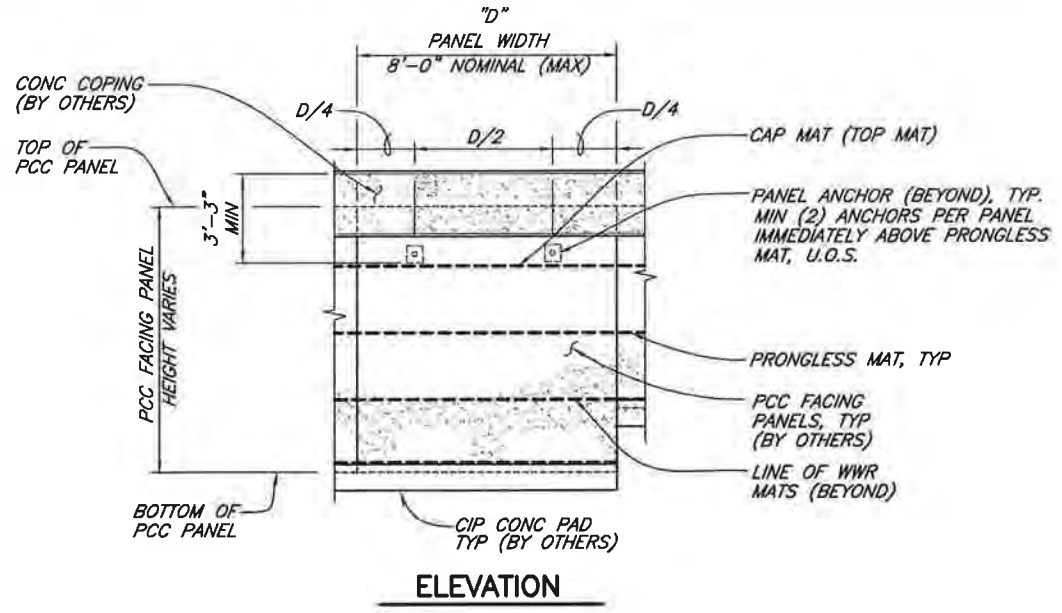
**DETAILS**

SHEET 7 OF 10  
DATE 8/30/12  
PROJ. NO. 012167

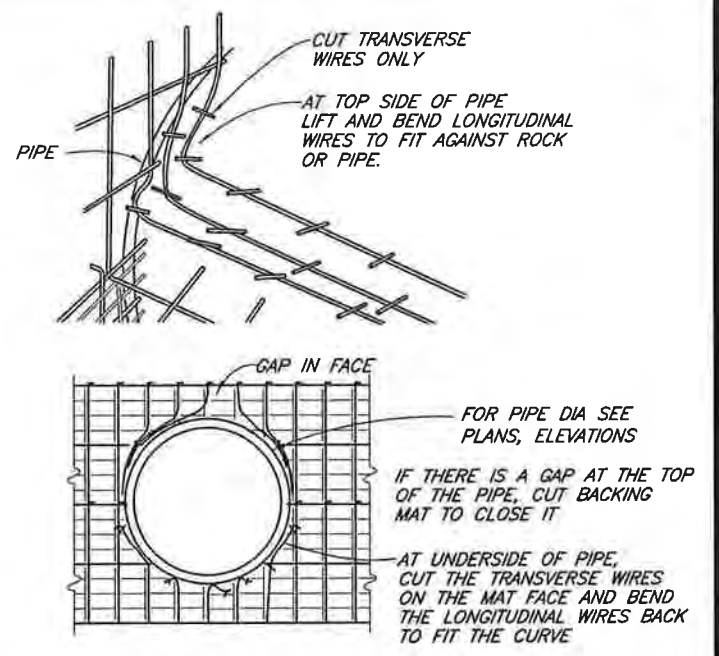
CAD: \\ZINC\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R3.DWG (HRW 120613 AE)



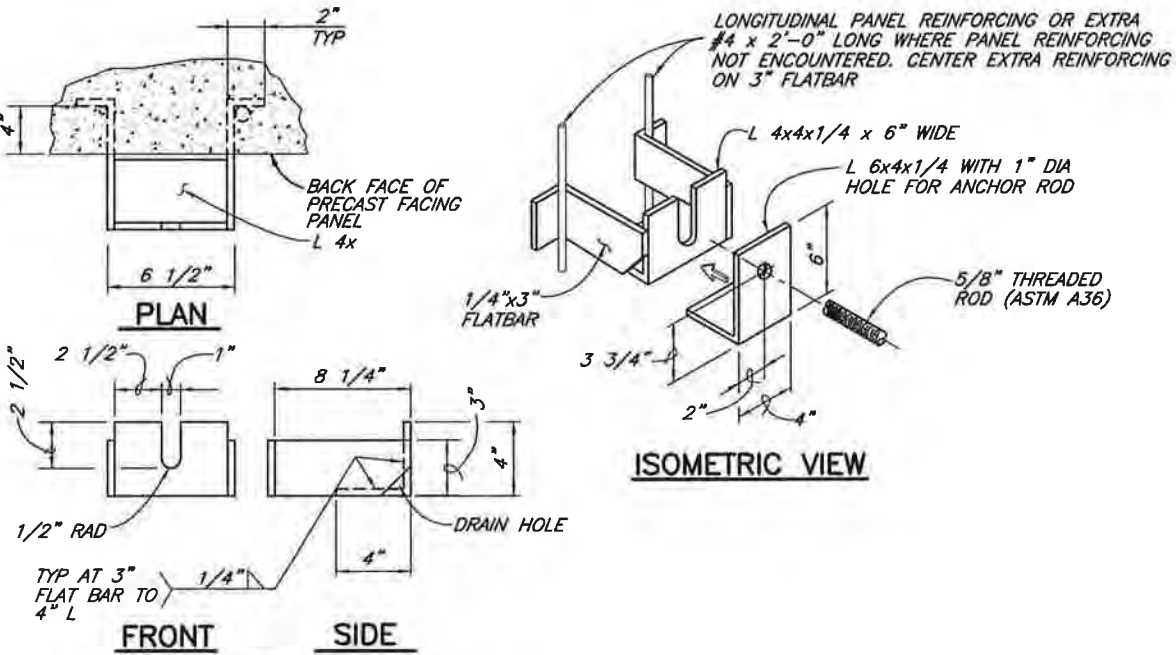
**DETAIL 1**  
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U.O.S. = UNLESS OTHERWISE STATED



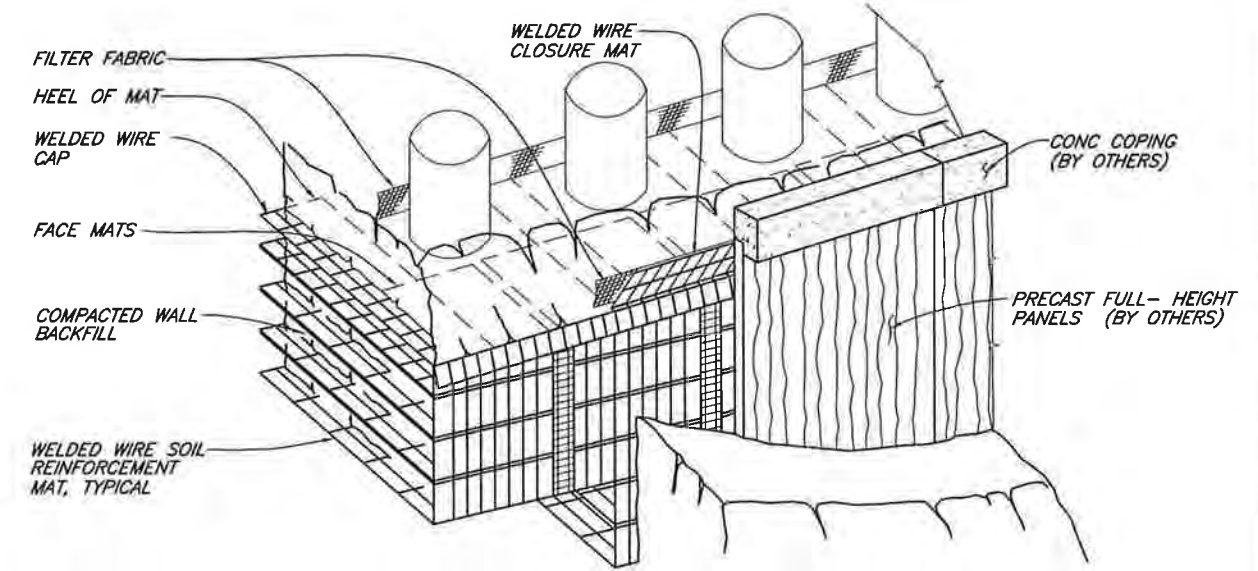
**PCC PANEL CONNECTION DETAIL**  
NO SCALE  
U.O.S. = UNLESS OTHERWISE STATED



**HORIZONTAL PENETRATION DETAIL**  
NO SCALE



**DETAIL 2**  
NO SCALE



**PICTORIAL ELEVATION**  
NO SCALE

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812 W. Wabasha Eureka, CA 95501  
TEL: (707) 441-8855 FAX: (707) 441-8877

**SM**

DESIGN	K/JN	APVD	K/JN
DR	NMD	CHK	K/JN

DATE: 10/8/12  
REVISION: Review comments

**DETAILS**

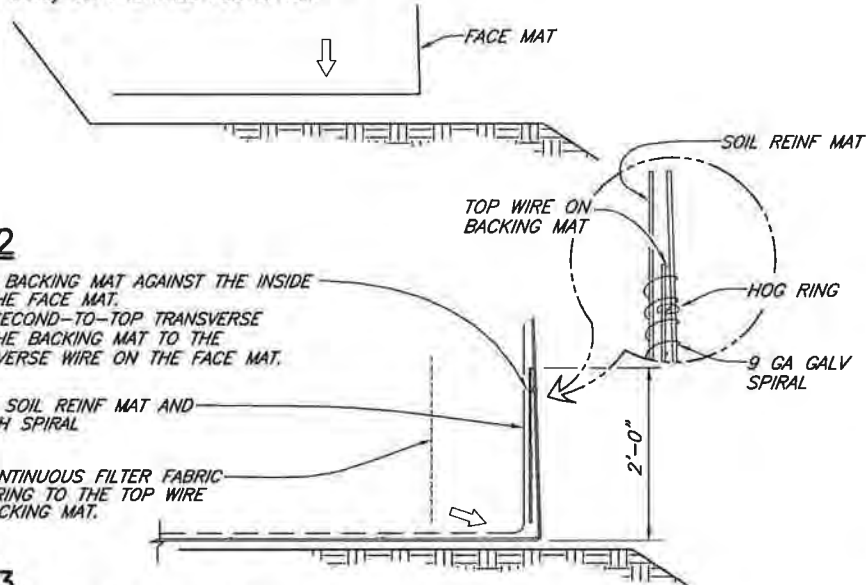
SHEET 8 OF 10  
DATE 8/30/12  
PROJ. NO. 012167

CAD: \\ZING\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R2.DWG (HRW 120613 AE)



**STEP 1**

PLACE THE FIRST COURSE OF SOIL REINFORCEMENT MATS (FACE MATS) ON PREPARED FOUNDATION.

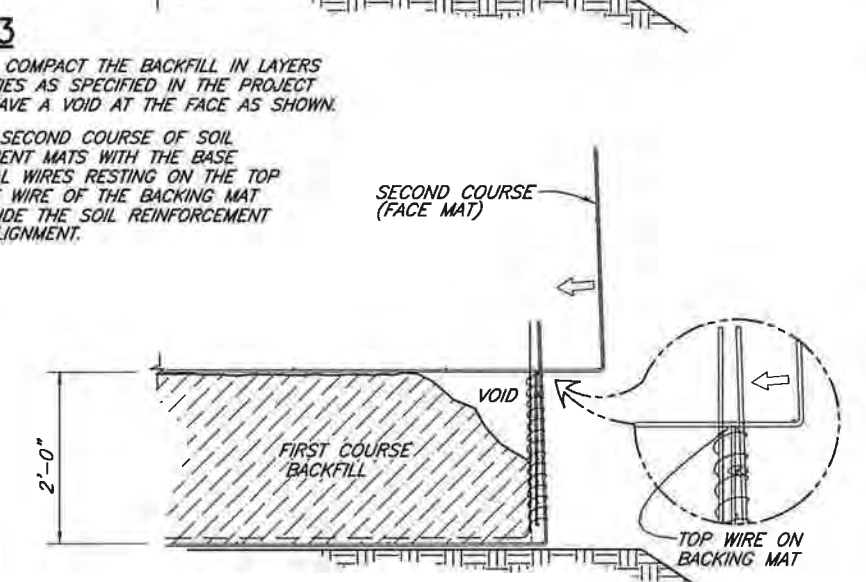


**STEP 2**

PLACE THE BACKING MAT AGAINST THE INSIDE FACE OF THE FACE MAT. CLIP THE SECOND-TO-TOP TRANSVERSE WIRE ON THE BACKING MAT TO THE TOP TRANSVERSE WIRE ON THE FACE MAT.

PLACE THE SOIL REINF MAT AND ATTACH WITH SPIRAL.

INSTALL CONTINUOUS FILTER FABRIC AND HOG-RING TO THE TOP WIRE ON THE BACKING MAT.



**STEP 3**

PLACE AND COMPACT THE BACKFILL IN LAYERS AND DENSITIES AS SPECIFIED IN THE PROJECT PLANS. LEAVE A VOID AT THE FACE AS SHOWN. PLACE THE SECOND COURSE OF SOIL REINFORCEMENT MATS WITH THE BASE LONGITUDINAL WIRES RESTING ON THE TOP TRANSVERSE WIRE OF THE BACKING MAT BELOW. SLIDE THE SOIL REINFORCEMENT MAT INTO ALIGNMENT.

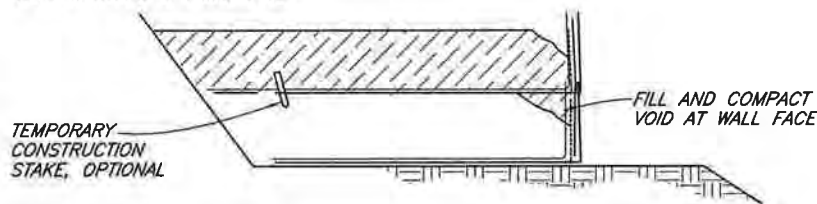
PLACE SOIL REINF MAT AND ATTACH WITH SPIRAL.



**NOTE:** ROCK FACING AND FACE MATS ONLY OCCUR AT PORTIONS OF WALLS AT ABUTMENTS

**STEP 5**

INSTALL THE FILTER FABRIC. PLACE AND COMPACT THE BACKFILL TO THE BASE ELEVATION OF THE NEXT MAT. REPEAT STEPS 3 THRU 5 TO THE TOP LIFT.



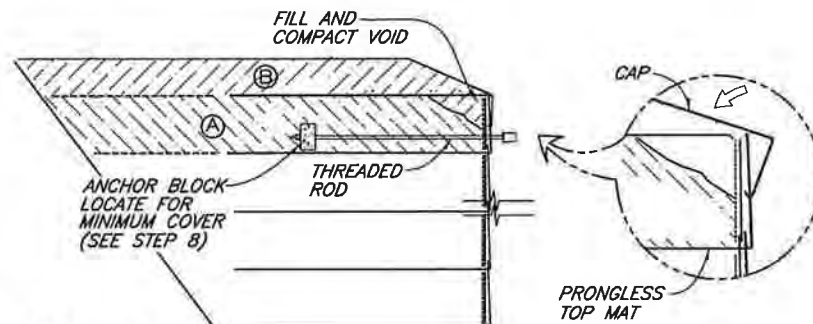
**STEP 6: TOP LIFT**

PLACE THE TOP LIFT PRONGLESS MAT, FACE MAT (WHERE OCCURS), AND BACKING MAT AND FILTER FABRIC. INSTALL ANCHOR BLOCKS AND THREADED RODS.

PLACE AND COMPACT BACKFILL IN AREA "A"

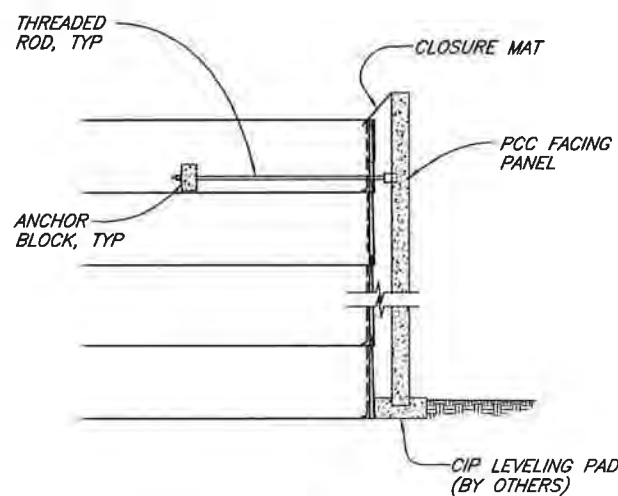
HOOK THE CAP OVER THE MIDDLE TRANSVERSE WIRE ON THE PRONGLESS MAT, AND ROTATE INTO PLACE.

BACKFILL "B" TO 1'-6" MINIMUM COVER OVER THE CAP.



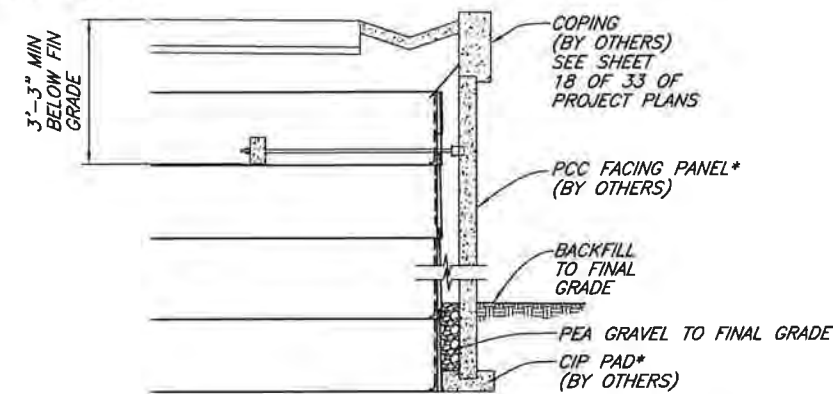
**STEP 7**

WAIT FOR SETTLEMENT PERIOD (PER CONTRACT SPECIFICATIONS) AND THEN POUR CIP LEVELING PAD TO ELEVATION SHOWN. INSTALL PCC FACING PANELS TO CIP PAD AND THREADED RODS FROM ANCHOR BLOCKS. PLACE CLOSURE MAT WITH BACKING MAT AND FILTER FABRIC.

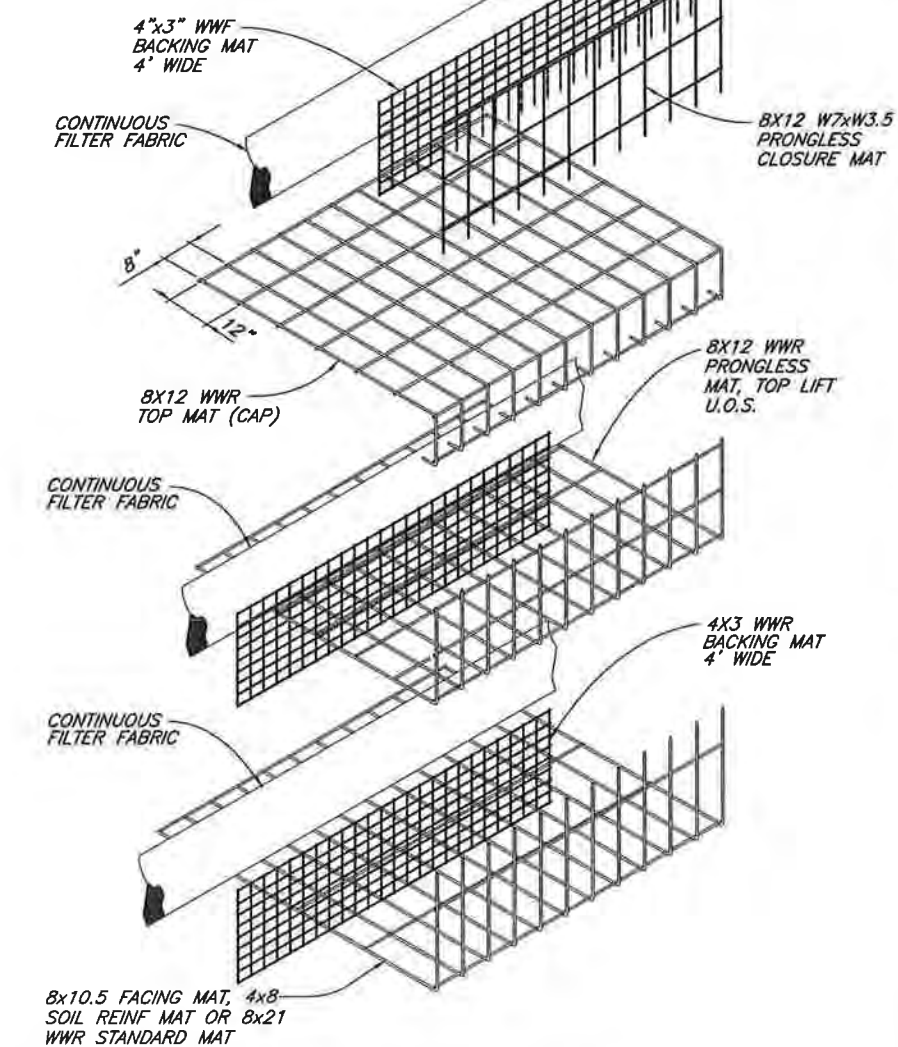


**STEP 8**

BACKFILL AT TOE TO FINAL GRADE. PLACE AND COMPACT BACKFILL AT CLOSURE MAT. FINISH TOP OF WALL PER PROJECT PLANS.



\* SEE PRECASTER SUBMITTAL FOR DETAILS



**WALL COMPONENTS**  
NO SCALE

**CONSTRUCTION SEQUENCE**  
NO SCALE

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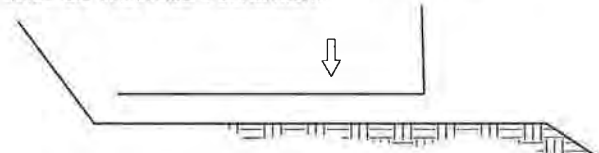


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CONSULTING ENGINEERS & GEOLOGISTS, INC.		812 W. Webster St. Eureka, CA 95501		TEL: (707) 441-8855 FAX: (707) 441-8877	
		K/J/N	K/J/N	BY	
		REVISION	DATE	REVIEW COMMENTS	
DESIGN	K/J/N	DR	M/M/D	CHK	K/J/N
APVD					
NO.	1	DATE	11/14/12	REVIEW COMMENTS	Review comments
NO.	2	DATE	10/18/12	REVIEW COMMENTS	Review comments
SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD IDAHO DEPARTMENT OF TRANSPORTATION MERIDIAN, IDAHO					
WALL COMPONENTS AND CONSTRUCTION SEQUENCE					
SHEET	9 OF 10		DATE	8/30/12	
PROJ. NO.	012167				

CAD: \\ZING\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R2.DWG (HRW 120613 AE)

**STEP 1**

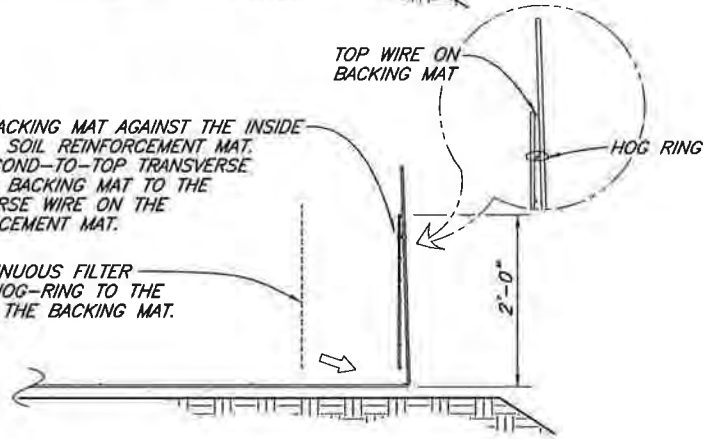
PLACE THE FIRST COURSE OF SOIL REINFORCEMENT MATS ON PREPARED FOUNDATION.



**STEP 2**

PLACE THE BACKING MAT AGAINST THE INSIDE FACE OF THE SOIL REINFORCEMENT MAT. CLIP THE SECOND-TO-TOP TRANSVERSE WIRE ON THE BACKING MAT TO THE TOP TRANSVERSE WIRE ON THE SOIL REINFORCEMENT MAT.

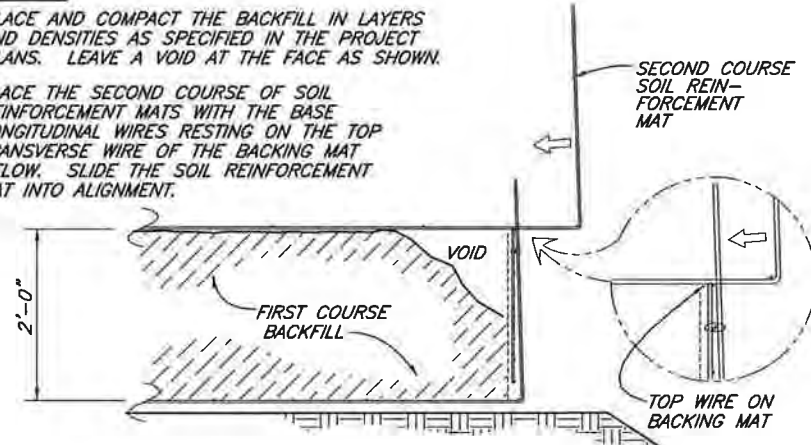
INSTALL CONTINUOUS FILTER FABRIC AND HOG-RING TO THE TOP WIRE ON THE BACKING MAT.



**STEP 3**

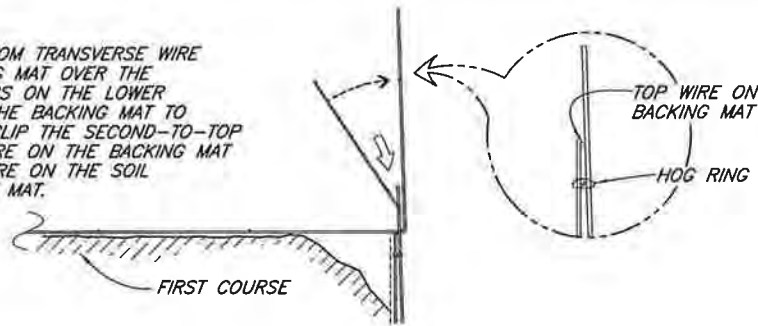
PLACE AND COMPACT THE BACKFILL IN LAYERS AND DENSITIES AS SPECIFIED IN THE PROJECT PLANS. LEAVE A VOID AT THE FACE AS SHOWN.

PLACE THE SECOND COURSE OF SOIL REINFORCEMENT MATS WITH THE BASE LONGITUDINAL WIRES RESTING ON THE TOP TRANSVERSE WIRE OF THE BACKING MAT BELOW. SLIDE THE SOIL REINFORCEMENT MAT INTO ALIGNMENT.



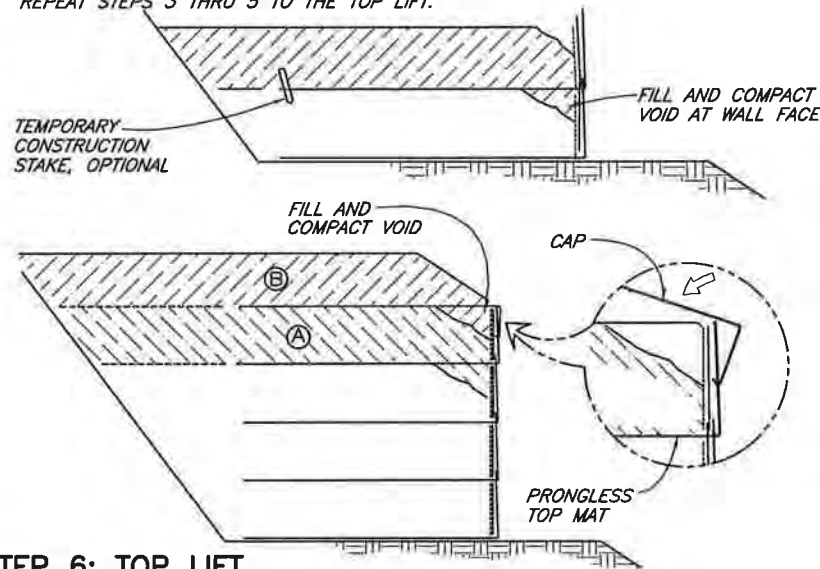
**STEP 4**

HOOK THE BOTTOM TRANSVERSE WIRE OF THE BACKING MAT OVER THE VERTICAL PRONGS ON THE LOWER MAT. ROTATE THE BACKING MAT TO VERTICAL AND CLIP THE SECOND-TO-TOP TRANSVERSE WIRE ON THE BACKING MAT TO THE TOP WIRE ON THE SOIL REINFORCEMENT MAT.



**STEP 5**

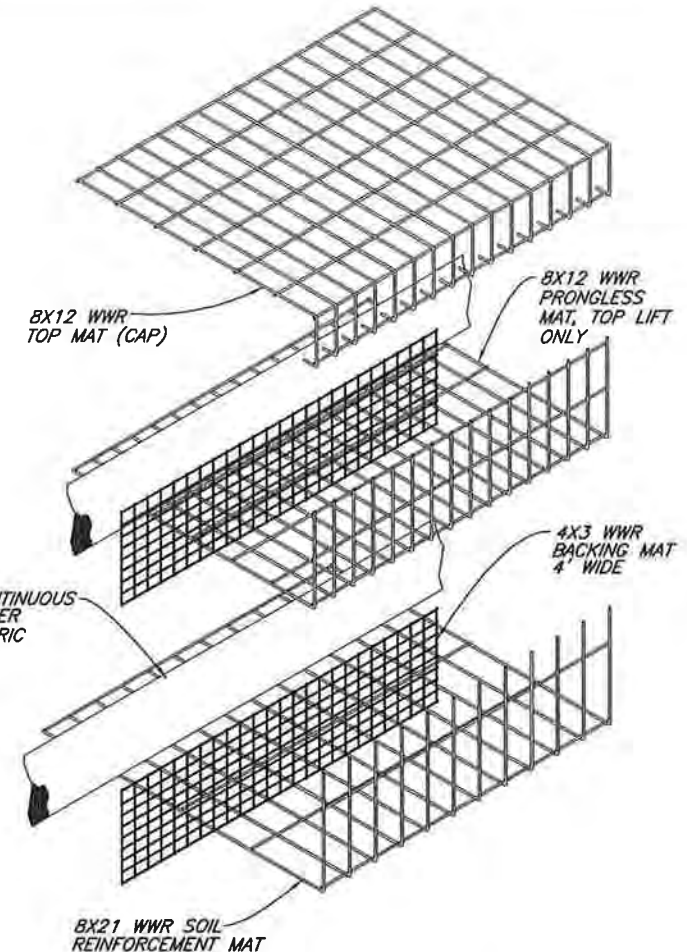
INSTALL THE FILTER FABRIC. PLACE AND COMPACT THE BACKFILL TO THE BASE ELEVATION OF THE NEXT MAT. REPEAT STEPS 3 THRU 5 TO THE TOP LIFT.



**STEP 6: TOP LIFT**

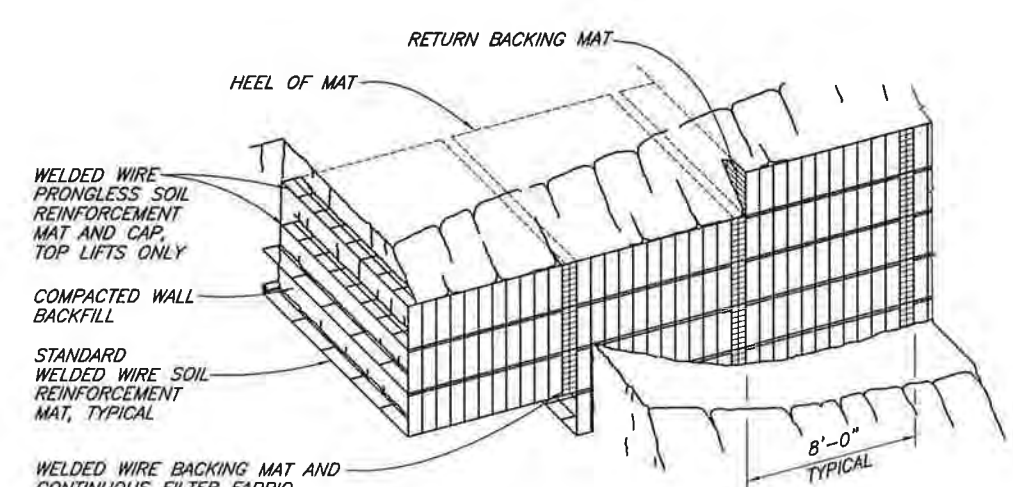
PLACE THE TOP LIFT PRONGLESS MAT, BACKING MAT AND FILTER FABRIC. PLACE AND COMPACT BACKFILL IN AREA "A". HOOK THE CAP OVER THE MIDDLE TRANSVERSE WIRE ON THE PRONGLESS MAT, AND ROTATE INTO PLACE. BACKFILL "B" TO 1'-6" MINIMUM COVER OVER THE CAP.

**CONSTRUCTION SEQUENCE**



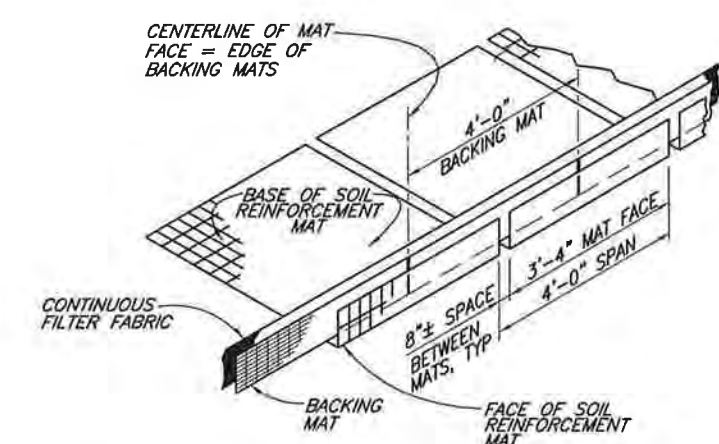
**WALL COMPONENTS**

NO SCALE



**PICTORIAL ELEVATION**

NO SCALE



**ASSEMBLED WALL COMPONENTS**

NO SCALE

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<b>SW</b>		
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DR	NMD	
CHK	K/JN	
APVD		
NO.		
DATE		
REVISION		
BY		
SH-16 OVER PHYLLIS CANAL & JOPLIN ROAD		
IDAHO DEPARTMENT OF TRANSPORTATION		
MERIDIAN, IDAHO		
<b>WALL COMPONENTS AND CONSTRUCTION SEQUENCE</b>		
SHEET	10 OF 10	
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CAD: \\ZING\PROJECTS\2012\012167-SH-16\DWGS\012167-SH-16-R2.DWG (HRW 120613 AE)